

Maryland Historical Trust Maryland Inventory of Historic Properties Form

Inventory No. CARR-1261

EASEMENT

1. Name of Property

(indicate preferred name)

historic Hampstead Train Station

other

2. Location

street and number 4035 Gill Avenue not for publication

city, town Hampstead vicinity

county Carroll

3. Owner of Property

(give names and mailing addresses of all owners)

name CSX Transportation, Inc.

street and number 500 Water Street, 15th Floor telephone (904) 359-3200

city, town Jacksonville state Florida zip code 32202-4423

4. Location of Legal Description

courthouse, registry of deeds, etc. liber folio

city, town tax map 500 tax parcel 2992 tax ID number 08032939

5. Primary Location of Additional Data

- ☐ Contributing Resource in National Register District
☐ Contributing Resource in Local Historic District
☐ Determined Eligible for the National Register/Maryland Register
☐ Determined Ineligible for the National Register/Maryland Register
☐ Recorded by HABS/HAER
☐ Historic Structure Report or Research Report at MHT
☒ Other: MHT Preservation Easement Property

6. Classification

Category

☐ district
☒ building(s)
☐ structure
☐ site
☐ object

Ownership

☐ public
☒ private
☐ both

Current Function

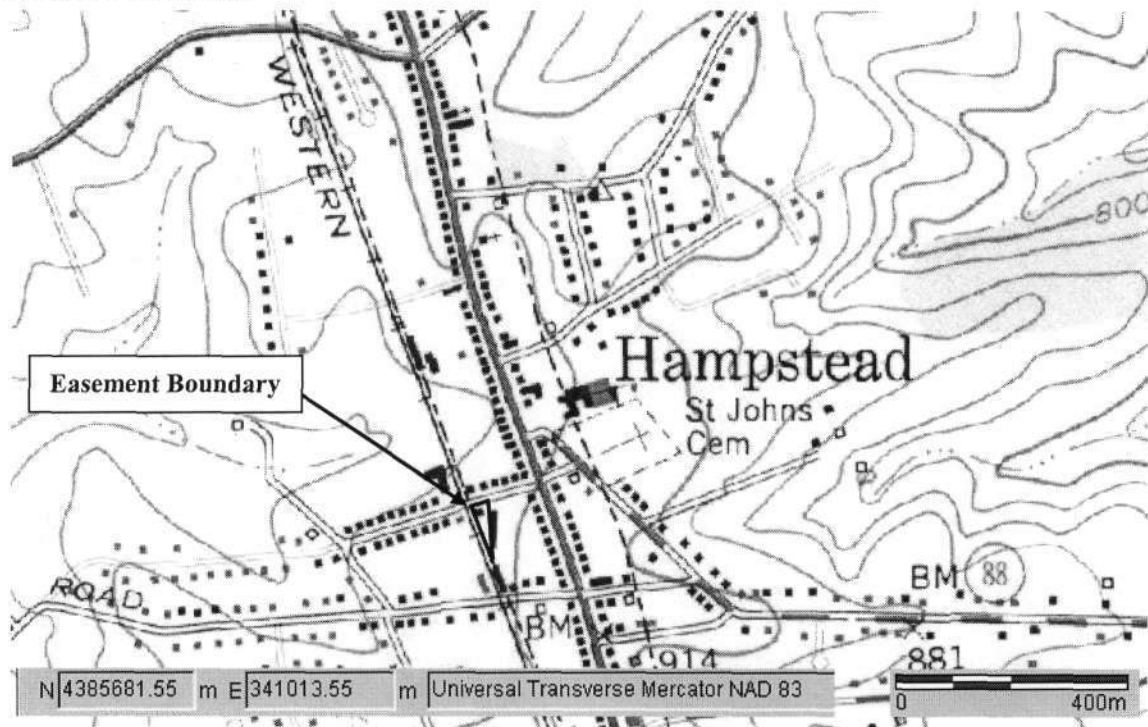
☐ agriculture
☒ commerce/trade
☐ defense
☐ domestic
☐ education
☐ funerary
☐ government
☐ health care
☐ industry
☐ landscape
☐ recreation/culture
☐ religion
☐ social
☒ transportation
☐ work in progress
☐ unknown
☐ vacant/not in use
☐ other:

Resource Count

| Contributing | Noncontributing |
|--------------|-----------------|
| 1 | buildings |
| | sites |
| | structures |
| | objects |
| | Total |

Number of Contributing Resources
previously listed in the Inventory

CARR-1261
Hampstead Train Station
4035 Gill Avenue
Hampstead Quadrangle



1998 Aerial Photo



SIGNED ORIGINAL ON FILE WITH THE M.H.T.
GRANTOR GRANTEE

**EXHIBIT B
PROPERTY DESCRIPTION**

**HAMPSTEAD TRAIN STATION
4035 GILL AVENUE
HAMPSTEAD, CARROLL COUNTY**

Beginning at a point on the south side of Gill Avenue (40 feet wide), said point also being on the first (1st) or North 74 degrees 08 minutes East 117.76 foot line of the land as described in a deed from Irvin S. Leister and Lena B. Leister, his wife, to The Western Maryland Railway Co., a body corporate of the State of Maryland, dated July 16, 1912 and recorded among the Land Records of Carroll County, Maryland in Liber 120, folio 147, at the distance of 39.38 feet measured along said line from an iron pipe found at the beginning thereof, thence leaving the place of beginning and running with and binding on the south side of aforesaid Gill Avenue and also running with and binding on the outline of the land as described in the aforesaid deed and as now surveyed by Stephen C. Barnhart, Registered Property Line Surveyor, #168, Viz:

- 1) North 81 degrees 15 minutes 00 seconds East 78.38 feet to a railroad spike now set at the beginning of the second (2nd) or South 05 degrees 29 minutes East 76.70 foot line of the land as described in the aforesaid deed, thence leaving the south side of Gill Avenue and continuing with the outline of the land as described in the aforesaid deed, viz:
 - 2) South 01 degrees 46 minutes 00 seconds West 76.70 feet to a rebar and cap now set, thence,
 - 3) South 07 degrees 30 minutes 00 seconds West 109.15 feet to a rebar and cap now set, thence,
 - 4) South 04 degrees 55 minutes 01 seconds West 64.46 feet to a rebar and cap now set at the beginning of the fifth (5th) line of the land as described in the aforesaid deed, thence leaving the outline of the land as described in the aforesaid deed and running for lines of a maintenance easement, as now established, through the land as described in said deed, of which the easement now being described is a part, the ten (10) following courses, viz:
 - 5) South 77 degrees 10 minutes 04 seconds West 1.27 feet to a point, said point being distant 30.00 feet, measured at right angles in a northeasterly direction from the centerline

of the existing CSX Transportation, Inc. railroad track, thence running parallel to and distant 30.00 feet, measured at right angles in a northeasterly direction from said centerline of track, viz:

6) North 12 degrees 49 minutes 56 seconds West 156.60 feet to a point intended to coincide with the roof overhang of the existing train station building, above, thence running to coincide with said overhang the seven (7) following courses, viz:

7) South 77 degrees 14 minutes 26 seconds West 14.02 feet to a point, thence,
8) North 12 degrees 45 minutes 34 seconds West 46.40 feet to a point, thence,
9) South 77 degrees 14 minutes 26 seconds West 2.50 feet to a point, thence,
10) North 12 degrees 45 minutes 34 seconds West 16.35 feet to a point, thence,
11) North 77 degrees 14 minutes 26 seconds East 2.50 feet to a point, thence,
12) North 12 degrees 45 minutes 34 seconds West 18.65 feet to a point and thence,
13) North 77 degrees 14 minutes 26 seconds East 13.92 feet to a point, thence leaving the coincidence with the aforesaid roof overhang, and again running parallel to and distant 30.00 feet measured at right angles in a northeasterly direction from the aforesaid centerline of track, viz:

14) North 12 degrees 49 minutes 56 seconds West 5.54 feet to the place of beginning. Containing 11, 244 square feet of 0.2581 acres of land, more or less.

Being a part of the holdings as indicated in a Certificate of Merger, filed December 16, 1988 and recorded among the land Records of Carroll County, Maryland in Book 36, page 788, by and between Western Maryland Railway Company, (a Maryland corporation) into CSX Transportation, Inc. (a Virginia corporation).

Also being a part of the land which by deed dated July 16, 1912 and recorded among the Land Records of Carroll County, Maryland in Liber 120, folio 147, was conveyed by Irvin S. Leister and Lena B. Leister, his wife, to The Western Maryland Railway Company, a body corporate of the State of Maryland.

Subject to an easement agreement by and between CSX Transportation, Inc., a Virginia corporation and Baltimore Gas and Electric Company, a corporation of the State of Maryland, dated December 17, 1991 and recorded among the Land Records of Carroll County, Maryland in Book 1322, page 725.

Being all that same land upon which CSX Transportation, Inc. conveyed a Deed of Easement to The Town of Hampstead dated January 5, 1998, and recorded at the Land Records of Carroll County in Liber 1993, page 0428, the interest in said maintenance easement thereafter being conveyed by The Town of Hampstead to the Hampstead Train Station Committee, Inc. in a Deed of Easement dated January 24, 2000 and recorded in the Land Records of Carroll County at Liber 2304, page 0845.

FEB 15 2003

21

Randall S Keirik
100 Community Pla
Crownsville Md.
21037

Excluded From Fee pursuant to the State of
Maryland Annotated Code, Tax Property Article 12-108(a)(ii).

EXP. FD. CURR. \$ 0.00
RECORDING FEE 0.00
TR. TAX STATE 0.00
TOTAL 0.00
Fee#0002 6000000000
LMS LMB BIR#457
Dec 03, 0902 09:05 AM

RCVD 06 DEC03'02 09:21

EXHIBIT A

**SCHEDULE
HAMPSTEAD TRAIN STATION
4035 GILL AVENUE
HAMPSTEAD, CARROLL COUNTY**

| | |
|---------------|--|
| PAGE 1 OF 11 | Schedule |
| PAGE 2 OF 11 | Site plan |
| PAGE 3 OF 11 | General view of the train station, looking southeast, with grain elevators visible in the rear. (15) |
| PAGE 4 OF 11 | North, or Gill Street, elevation showing the passenger end of the station. (16) |
| PAGE 5 OF 11 | South elevation showing the freight end of the station. (13) |
| PAGE 6 OF 11 | South and west elevations looking northeast across the railroad tracks. (14) |
| PAGE 7 OF 11 | South and east elevations looking to the northwest. (12) |
| PAGE 8 OF 11 | Interior view, looking south from the passenger area into the raised freight section. (17) |
| PAGE 9 OF 11 | Interior view, looking north from the freight area into the passenger section. (18) |
| PAGE 10 OF 11 | Contact sheet prints |
| PAGE 11 OF 11 | Identification of contact sheet prints |

HAMPSTEAD TRAIN STATION
HAMPSTEAD, CARROLL COUNTY

Schedule

EASEMENT EXHIBIT NO. A, Page 1 of 11

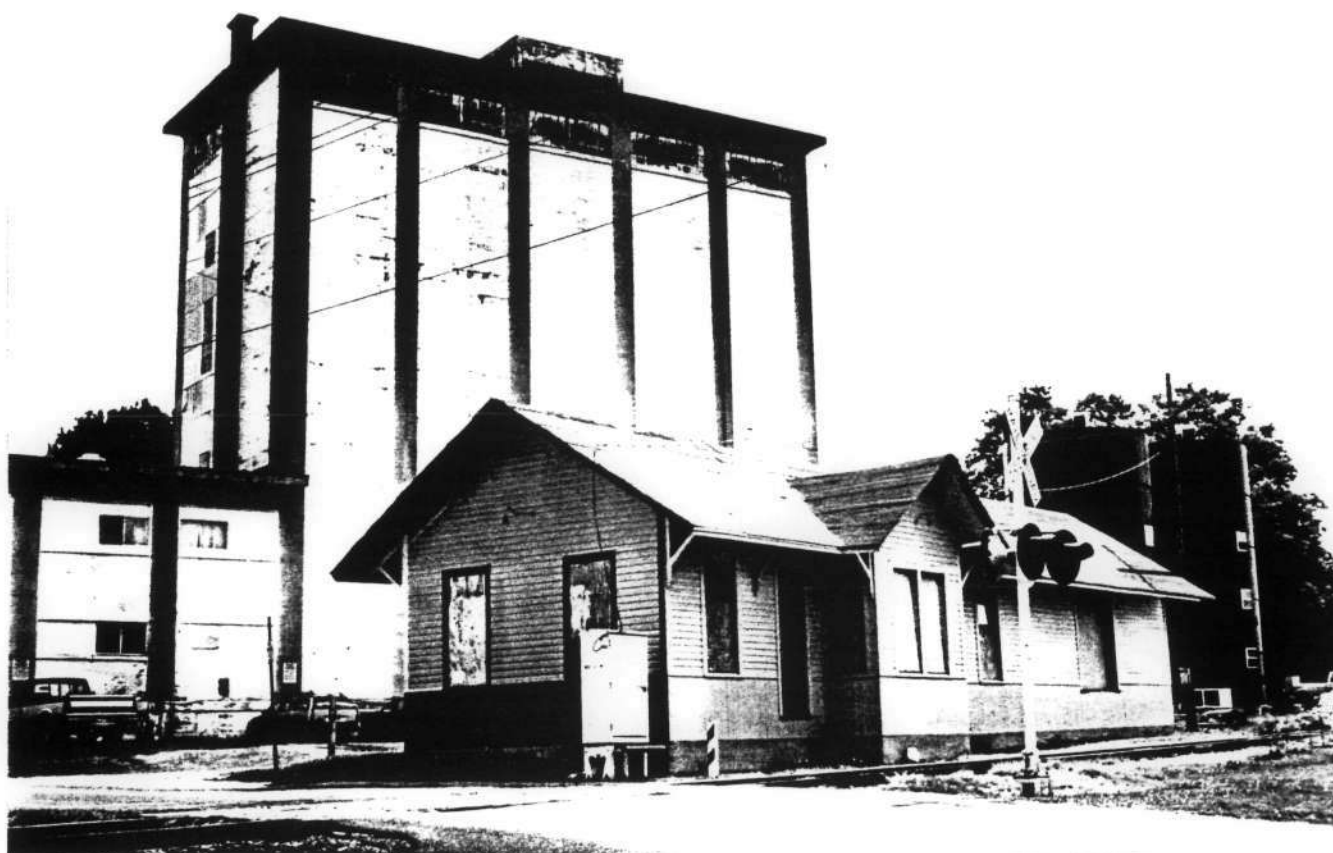
PREPARED BY: RDS SEPTEMBER, 2002
MARYLAND HISTORICAL TRUST

SIGNED ORIGINAL ON FILE WITH THE M.H.T.

GRANTOR

GRANTEE

GRANTEE



HAMPSTEAD TRAIN STATION
HAMPSTEAD, CARROLL COUNTY

General view of the train station, looking southeast, with
grain elevators visible in the rear.

EASEMENT EXHIBIT NO. A, Page 3 of 11

PREPARED BY: RDS SEPTEMBER, 2002
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GRANTOR

GRANTEE



HAMPSTEAD TRAIN STATION
HAMPSTEAD, CARROLL COUNTY

North, or Gill Street, elevation showing the passenger end
of the station.

EASEMENT EXHIBIT NO. A, Page 4 of 11

PREPARED BY: RDS SEPTEMBER, 2002
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GRANTOR

GRANTEE



HAMPSTEAD TRAIN STATION
HAMPSTEAD, CARROLL COUNTY

South elevation showing the freight end of the station.

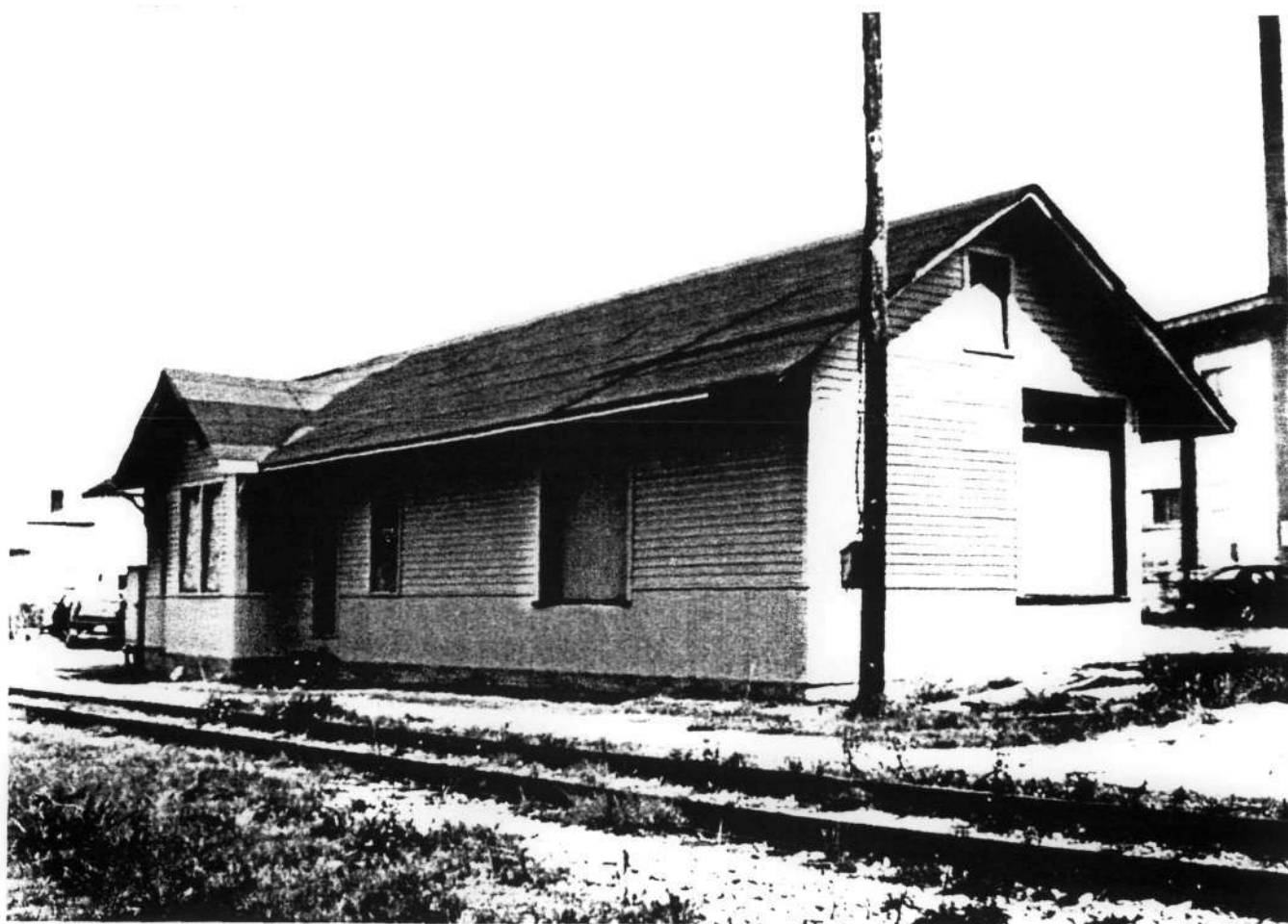
EASEMENT EXHIBIT NO. A, Page 5 of 11

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GRANTOR

GRANTEE



HAMPSTEAD TRAIN STATION
HAMPSTEAD, CARROLL COUNTY

South and west elevations looking northeast across the
railroad tracks.

EASEMENT EXHIBIT NO. A, Page 6 of 11

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GRANTOR

GRANTEE



HAMPSTEAD TRAIN STATION
HAMPSTEAD, CARROLL COUNTY

South and east elevations looking to the northwest.

EASEMENT EXHIBIT NO. A, Page 7 of 11

PREPARED BY: RDS SEPTEMBER, 2002
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SIGNED ORIGINAL ON FILE WITH THE M.H.T.

GRANTOR

GRANTEE

Easement

CARR-1261

Gill Avenue, Hampstead

Summary:

When the Hampstead Railroad Station was first inventoried in 1986, it was noted that the station was built in 1912, according to Chessie System officials, but that local residents believed it to be a nineteenth-century structure. Additional research proves that the 1912 date is accurate. A study of the country railroad station in America, published in 1988, concluded that the Hampstead depot "... contains racially segregated waiting rooms" The small black population of the region makes it unlikely that these rooms were to segregate the races, especially since the rooms were similar in size and finish, and were apparently connected at one end, where they shared access to a single ticket window. The only difference between the rooms was that the one on the north end was heated, while the waiting room between the agent's office and the freight depot on the south end was not. It seems likely that the heated room was for women to wait in, while the other room, which would have seen traffic from the agent passing between his office and the freight room, would have been a men's waiting room. Additional research suggests that the two rooms were meant to segregate the sexes.

**Maryland Historical Trust
State Historic Sites Inventory Form**

MARYLAND INVENTORY OF
HISTORIC PROPERTIES

Survey No. **CARR- 1261**

Magi No.

DOE yes no

Easement

1. Name (indicate preferred name)

historic Hampstead Railroad Station

and/or common

2. Location

street & number Gill Avenue @ Railroad tracks not for publication

city, town Hampstead vicinity of congressional district

state Maryland county Carroll

3. Classification

| Category | Ownership | Status | Present Use |
|-----------------------|----------------------------|-----------------------------|---|
| <u> </u> district | <u> </u> public | <u> </u> occupied | <u> </u> agriculture <u> </u> museum |
| <u> </u> building(s) | <u> </u> private | <u> </u> unoccupied | <u> </u> commercial <u> </u> park |
| <u> </u> structure | <u> </u> both | <u> </u> work in progress | <u> </u> educational <u> </u> private residence |
| <u> </u> site | Public Acquisition | Accessible | <u> </u> entertainment <u> </u> religious |
| <u> </u> object | <u> </u> in process | <u> </u> yes: restricted | <u> </u> government <u> </u> scientific |
| | <u> </u> being considered | <u> </u> yes: unrestricted | <u> </u> industrial <u> </u> transportation |
| | <u> </u> not applicable | <u> </u> no | <u> </u> military <u> </u> other: |

4. Owner of Property (give names and mailing addresses of all owners)

name Town of Hampstead

street & number 1034 South Carroll Street telephone no.: (410) 239-7408

city, town Hampstead state and zip code Maryland 21074

5. Location of Legal Description

courthouse, registry of deeds, etc. Courthouse Annex liber

street & number 55 North Court Street folio

city, town Westminster state Maryland

6. Representation in Existing Historical Surveys

title MHT State Historic Sites Inventory Form

date June 1986 federal state county local

depository for survey records MHT

city, town Crownsville state Maryland

7. Description

Survey No. CARR-1261

Condition

☐ excellent
☐ good
☐ fair

☒ deteriorated
☐ ruins
☐ unexposed

Check one

☐ unaltered
☒ altered

Check one

☒ original site
☐ moved date of move _____

Prepare both a summary paragraph and a general description of the resource and its various elements as it exists today.

SEE ATTACHED

8. Significance

Survey No. CARR-1261

| Period | Areas of Significance—Check and justify below | | | |
|---|--|---|---|---|
| <input type="checkbox"/> prehistoric | <input type="checkbox"/> archeology-prehistoric | <input type="checkbox"/> community planning | <input type="checkbox"/> landscape architecture | <input type="checkbox"/> religion |
| <input type="checkbox"/> 1400–1499 | <input type="checkbox"/> archeology-historic | <input type="checkbox"/> conservation | <input type="checkbox"/> law | <input type="checkbox"/> science |
| <input type="checkbox"/> 1500–1599 | <input type="checkbox"/> agriculture | <input type="checkbox"/> economics | <input type="checkbox"/> literature | <input type="checkbox"/> sculpture |
| <input type="checkbox"/> 1600–1699 | <input checked="" type="checkbox"/> architecture | <input type="checkbox"/> education | <input type="checkbox"/> military | <input type="checkbox"/> social/humanitarian |
| <input type="checkbox"/> 1700–1799 | <input type="checkbox"/> art | <input type="checkbox"/> engineering | <input type="checkbox"/> music | <input type="checkbox"/> theater |
| <input type="checkbox"/> 1800–1899 | <input type="checkbox"/> commerce | <input type="checkbox"/> exploration/settlement | <input type="checkbox"/> philosophy | <input type="checkbox"/> transportation |
| <input checked="" type="checkbox"/> 1900– | <input type="checkbox"/> communications | <input type="checkbox"/> industry | <input type="checkbox"/> politics/government | <input checked="" type="checkbox"/> other (specify) |
| | | <input type="checkbox"/> invention | | |

Specific dates 1912

Builder/Architect

check: Applicable Criteria: ☐ A ☐ B ☒ C ☐ D
and/or

Applicable Exception: ☐ A ☐ B ☐ C ☐ D ☐ E ☐ F ☐ G

Level of Significance: ☐ national ☐ state ☒ local

Prepare both a summary paragraph of significance and a general statement of history and support.

SEE ATTACHED

9. Major Bibliographical References

Survey No. CARR-1261

SEE ATTACHED

10. Geographical Data

Acreage of nominated property .626 A.

Quadrangle name Hampstead

Quadrangle scale 1:24000

UTM References do NOT complete UTM references

| | | | | |
|---|------|---------|-------|-----|
| A | Zone | Easting | North | ing |
| | | | | |

| | | | | |
|---|------|---------|-------|-----|
| B | Zone | Easting | North | ing |
| | | | | |

| | | | | |
|---|------|---------|-------|-----|
| C | Zone | Easting | North | ing |
| | | | | |

| | | | | |
|---|------|---------|-------|-----|
| D | Zone | Easting | North | ing |
| | | | | |

| | | | | |
|---|------|---------|-------|-----|
| E | Zone | Easting | North | ing |
| | | | | |

| | | | | |
|---|------|---------|-------|-----|
| F | Zone | Easting | North | ing |
| | | | | |

| | | | | |
|---|------|---------|-------|-----|
| G | Zone | Easting | North | ing |
| | | | | |

| | | | | |
|---|------|---------|-------|-----|
| H | Zone | Easting | North | ing |
| | | | | |

Verbal boundary description and justification

List all states and counties for properties overlapping state or county boundaries

| state | code | county | code |
|-------|------|--------|------|
|-------|------|--------|------|

| state | code | county | code |
|-------|------|--------|------|
|-------|------|--------|------|

11. Form Prepared By

name/title Kenneth M. Short, Historic Planner

organization Carroll County Planning & Development date May 18, 1999

street & number 225 North Center Street telephone (410) 386-2145

city or town Westminster state Maryland

The Maryland Historic Sites Inventory was officially created by an Act of the Maryland Legislature to be found in the Annotated Code of Maryland, Article 41, Section 181 KA, 1974 supplement.

The survey and inventory are being prepared for information and record purposes only and do not constitute any infringement of individual property rights.

return to: Maryland Historical Trust
Shaw House
21 State Circle
Annapolis, Maryland 21401
(301) 269-2438

MARYLAND HISTORICAL TRUST
DHCP/DHCD
100 COMMUNITY PLACE
CROWNSVILLE, MD 21032-2023
514-7600

MARYLAND HISTORICAL TRUST
STATE HISTORIC SITES INVENTORY FORM

Survey No.CARR-1261

Description

Section 7 Page 1

The Hampstead Railroad Station is located at the southeast corner of the intersection of Gill Avenue and the CSX railroad tracks in Hampstead in Northeast Carroll County Maryland. The Station is a one story 6 bay x 2 bay frame structure with a concrete foundation and a gable roof that had slate, but is now in the process of being rebuilt. The roof has a north/south ridge. The siding has beaded-edge-in-center vertical boards below a railing that matches the chair rail in the interior, above this railing is German siding with corner boards that are fastened with wire nails. The siding is painted light grey with a maroon trim, the windows are boarded up but are 2/2 double-hung sash and all of the doors have five lying panels and have one-light transom above. The station has over hanging eaves with brackets and a beaded-edge-in-center board soffit.

On the west elevation from north to south, there is a typical window, a typical door, projecting bay with paired typical windows and with a typical window on both the north and south elevations of the bay. The bay has a gable roof with an east/west ridge and stick work barge boards. South of the bay is a typical door, a typical window and beaded-edge-in-center vertical board freight door on rollers with a large transom above that is boarded up. The south elevation has a typical freight door and transom set to the east and a small 2/2 sash and a gable end. A portion of the original stick work barge board survives in the gable end. The east elevation has from south to north a typical freight door and transom, a typical window, a typical door, typical window, typical door and typical window at the north. The north elevation has two typical windows and stick work barge board in the gable end.

The interior has a large freight room at the south end and three rooms in the north end, the original arrangement consisted of north and south waiting rooms that were connected on the east and a station agents office in the west center section where the bay is. The east wall of the office was taken down and the north and south walls of the office extended to enlarge it and separate the north and south waiting rooms. The flooring throughout is 2 1/4 inch wide and runs north/south, the chair rail has a bull nose shelf with a taurus covetto bead and ogee below. The architrave is symmetrical and has a bead in the center with an ovolo to each side and a bead on each outer corner. There are bulls-eye corner blocks. The walls have beaded edge and center vertical board half wainscot below the chair rail and horizontal beaded-edge-in-center boards above the chair rail. The ceilings also had beaded-edge-in-center boards but most of this was removed when the roof framing was replaced.

The north room is painted a medium gray overtop of a light gray overtop of varnish below the chair rail. The chair rail and architrave are painted to match. Above the chair rail the wood is painted light gray over varnish. The architrave appears to be painted a medium gray over varnish. On the south elevation set to the west is a door with one light over three lined panels. This door

MARYLAND HISTORICAL TRUST
STATE HISTORIC SITES INVENTORY FORM

Survey No.CARR-1261

Description

Section 7 Page 2

leads to the center room or office. It originally swung into the center room but was moved to swing out. It is hung on metal plate butt hinges that are face screwed to the architrave and there are no mortices on the jam. It has a mortice lock with a stamped metal foliate escutcheon and knob. The door is painted light gray on the rails and stiles and medium gray on the panels. Beneath the paint is varnish. The exterior doors are painted to match this door.

The center room or office is painted light green above the chair rail overtop of a flesh color which is overtop of a grass green which is overtop of a medium olive green which is on top of varnish. Below the chair rail the walls are painted a light green on top of a flesh color overtop of a dark black green. The architrave is painted a dark gray green over a flesh tone over a dark black green over varnish. The ceiling is painted white over a putty color overtop of a light olive green. This appears to have been varnished originally as well. The door on the north elevation set to the west which leads to the north waiting room has rails and stiles that are painted a dark gray green over a flesh tone over a grass green over a medium olive green over a light green on top of varnish. The panels are a light green over flesh over a dark forest green over a medium mustard brown over varnish. On the north elevation east of the door is a brick chimney that is enclosed with the same siding as the rest of the room. It has a stove pipe hole on the north side for a stove that must have stood in the north waiting room. On the west elevation was a built in desk that was open in the center for the agents legs, part of the desk survives in the northwest corner, part has been removed and carried outside. The southwest corner of this room has been rebuilt and there was apparently a door on the south elevation that led to the south waiting room. There is a cellar below this room. The east ends of the north and south walls have plywood with battens covering the joints, there is chair rail and wainscot below it at the bottom of the walls that appears to have been used when the original eastern wall was taken down. The eastern section of the south elevation has a ticket window with typical architrave, the window slides up into the wall with a slide bolt to hold it in place. The window has a two light sash, these window was probably reused and moved from its original location, likely on the east elevation.

The south waiting room is a mirror image of the north room, the whole room is painted a flesh color over a light gray green or olive green over top of what appears to be varnish. The ceiling is painted white over a light gray green or olive green. There is a cornice molding at the ceiling fastened with wire nails. The south elevation to the west has a doorway that leads to the south freight room, there are three steps up inside of the door. The flooring in this room is 2" thick butted boards that are 6" or 12" wide and run north/south, the walls are sheathed half the way up with circular sawn half lapped boards. The studs here are circular sawn and are 1 3/4 inches by 5 1/2 inches. There is rough shelving built in on the north elevation to the east and there

MARYLAND HISTORICAL TRUST
STATE HISTORIC SITES INVENTORY FORM

Survey No. CARR-1261

Description

Section 7 Page 3

is a ladder on the north elevation to the attic area. Roof framing survives here and there is a ridge pole, the joists are lapped and nailed to the rafters and there are small dimension diagonal braces nailed from near the center of the rafters to near the center of the joists. The freight doors are hung on interior rollers.

Contributing Resources 1.

Significance

Section 8 Page 1

When the Hampstead Railroad Station was first inventoried in 1986, it was noted that the station was built in 1912, according to Chessie System officials, but that local residents believed it to be a nineteenth-century structure. Additional research proves that the 1912 date is accurate. A study of the country railroad station in America, published in 1988, concluded that the Hampstead depot "... contains racially segregated waiting rooms" The small black population of the region makes it unlikely that these rooms were to segregate the races, especially since the rooms were similar in size and finish, and were apparently connected at one end, where they shared access to a single ticket window. The only difference between the rooms was that the one on the north end was heated, while the waiting room between the agent's office and the freight depot on the south end was not. It seems likely that the heated room was for women to wait in, while the other room, which would have seen traffic from the agent passing between his office and the freight room, would have been a men's waiting room. Additional research suggests that the two rooms were meant to segregate the sexes.

This property was one of many owned by Charles M. Murray at his death in 1910, and held a grain elevator operated by Switzer and Bixler. Murray's son, acting as trustee, sold the property to Irvin Leister in 1910. The 1911 Sanborn map shows that there was a 2 ½ story grain elevator on the site, with a one-story passenger depot on the north end and a one-story freight house on the south end. Switzer & Bixler continued to operate the elevator. In July, 1912, Leister sold .626 acres, the land where the elevator was, to the Western Maryland Railway Company for \$1000. Several conditions were placed on the sale, including that the land could only be used for railroad functions, that Leister would continue to have the use of the switch, and that Leister "reserves the buildings including the foundation stone on the land hereby conveyed with the privilege of removing the same within fifty days from the date of this deed"

Leister had probably already started construction of the new mill (Carr-1262). By early August a correspondent could write, "Every vestige of the old depot and freight shed has been removed and there is now nothing to hinder the Western Maryland from building a decent depot and a good freight house at Hampstead." Taking down the old building proved easier than erecting the new one. A group of Hampstead residents petitioned the Public Service Commission to force the railway to "... erect a proper station" The petitioners complained that there was "... no provision for lavatories or separate rooms for men and women , " criticizing the railway's plans as a "one-horse" depot. The hearing was held on 13 September, and on 26 September the Western Maryland submitted new plans to the Commission, remedying the problems. The Commission accepted the proposed separate waiting rooms and ordered "separate closets for men and women for sanitary purposes, at about the distance of two hundred (200) feet in the rear of the said station." Another concern had been that the freight house would interfere with traffic on Railroad Avenue, so the Commission ordered that the freight depot portion of the station be located away from the road.

MARYLAND HISTORICAL TRUST
STATE HISTORIC SITES INVENTORY FORM

Survey No.CARR-1261

Significance

Section 8 Page 2

The concern about the local railroad station on the part of the citizens of Hampstead is not unusual. With the coming of the railroad, the local depot became a major gateway to the town, and residents of many cities and towns, of all sizes, wanted the station to make the best possible expression about the quality of the town. The station became a source of pride for many towns, and in the case of Hampstead is considered important enough that the town has recently acquired the building and is in the process of rehabilitating it.

Geographic Organization:
Piedmont

Chronological/Developmental Periods:
Industrial/Urban Dominance A.D. 1870-1930

Historic Period Theme:
Architecture/Transportation

Resource Type:
Railroad Station

MARYLAND HISTORICAL TRUST
STATE HISTORIC SITES INVENTORY FORM

Survey No. CARR-1261

Bibliography

Section 9 Page 1

Carroll County Land Records

Sanborn Fire Insurance Maps, "Hampstead, 1911, 1924

Grant H. Roger, and Charles W. Boh: *The Country Railroad Station in America*. Sioux Falls, S.D.: The Center for Western Studies, Augustana College, 1988, p. 64

Western Demo. Adv., 9 Aug. 1912, p. 3, c1; 16 Aug. 1912, p. 5, c3; 6 Sept. 1912, p. 7, c2

Report of the Public Service Commission of Maryland for the year 1912. Balto.: Sun Book and Job Printing Office, Inc., 1913. Case no. 428, pp. 302-3

CARR - 1261 & 1262
Hampstead Mill & Railroad Station

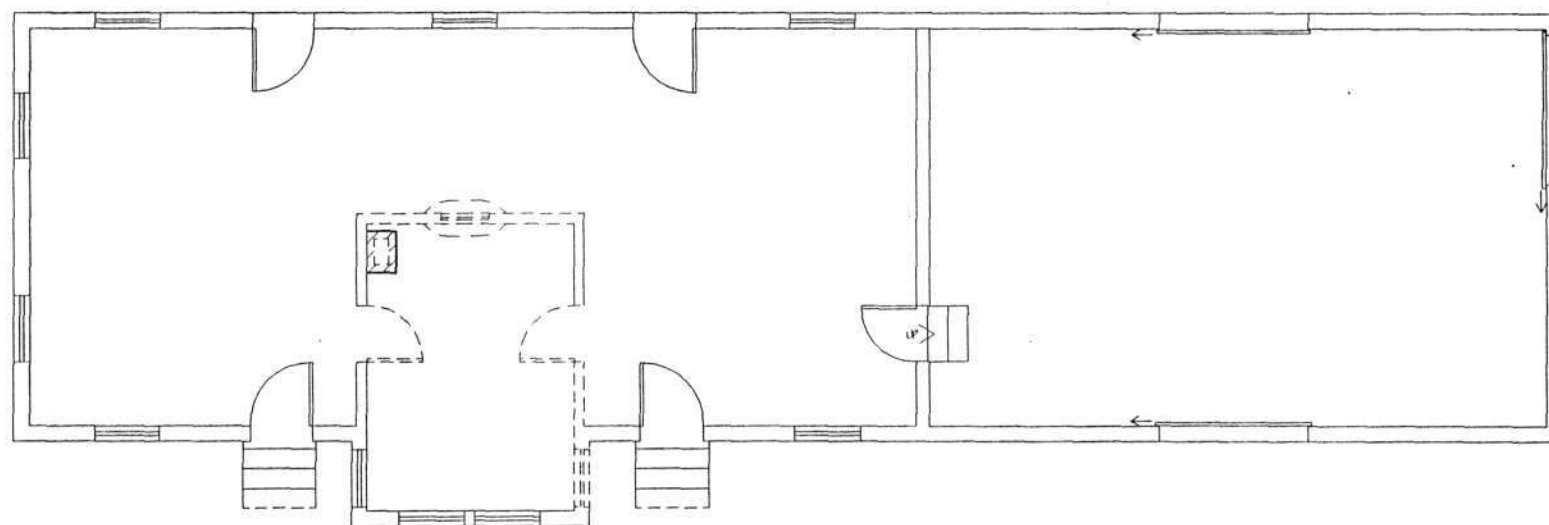
CHAIN OF TITLE

| GRANTOR | HOME COUNTY | GRANTEE | HOME COUNTY | DATE | LIBER | FOLIO | TRANS- ACTION | COMMENTS |
|---|----------------|--|----------------|---------------|---------|-------|------------------|--|
| Ralph Hoffman assignee of mortgage | | Josue C. Laredo & Christina I. (wf) | | 5 Dec. 1980 | LWS/779 | 759 | Deed fee simple | 4 continuous lots 1. 2 3 .099 ac. 4. .064 ac. \$200,000.00 Equity Dockett #16800 |
| Pride of the Valley, Inc. - Dela Corp. | | North Carroll Investment Co., MD Corp. | | 11 Jun. 1971 | CCC/489 | 159 | Deed fee simple | 4 lots |
| Valley Feed & Supply - NY Corp. | | Pride of the Valley, Inc. - Dela. Corp. | | 30 Sept. 1966 | CCC/419 | 448 | Deed fee simple | 4 lots |
| Quaker City Flour Mill, Inc. - Dela. Corp. | | Belt Seed Co., Inc. - MD Corp. | | 13 Jul. 1964 | CCC/376 | 682 | Deed fee simple | 4 lots |
| Western Maryland R. R. | | Quaker City Mills | | 15 Sept. 1961 | 340 | 10 | | Parcel 4 |
| Manchester Bank | | Quaker City Flour Mills, Inc. - Dela. Corp. | | 15 Apr. 1939 | LDM/170 | 292 | Deed fee simple | 2 parcels |
| John B. Baker, assignee of mortgage (Star Milling Co., Inc. - VA Corp.) | Carroll | Manchester Bank | | 9 Mar. 1939 | LDM/170 | 224 | Deed fee simple | 48,267 sq. ft. |
| William C. Miller, Pres., J. Frank Switzer, Sec. Hampstead Milling - MD Corp. | | Star Milling Cor. - VA Corp. & A. A. Roudabush, Shenandoah, VA | | 6 Jul. 1935 | EMM/162 | 345 | Deed, ? | a. 19,921 sq. ft. see 4 deeds below b. 28,346 sq. ft. |
| Irvin S. Leister & wf. Lena B. | Carroll | Malko Milling & Lighting Co. - MD Corp. | | 31 Mar. 1921 | EOC/138 | 252 | Deed fee simple | Murray's Division, lot 3 1+ ac. - .626 ac. sold to WMRR 16 Jul. 1912, lot 10 - 28,890 sq. ft. |

CARR - 1261 & 1262
Hampstead Mill & Railroad Station

CHAIN OF TITLE

| GRANTOR | HOME COUNTY | GRANTEE | HOME COUNTY | DATE | LIBER | FOLIO | TRANS- ACTION | COMMENTS |
|----------------------------|----------------|------------------------------|----------------|-----------------|---------|-------|------------------|---|
| Agnes A. Murray | | Malko Milling & Lighting Co. | | 31 Mar. 1921 | EOC/138 | 254 | | |
| J. Frank Switzer | | Malko Milling & Lighting Co. | | 31 Mar. 1921 | EOC/138 | 255 | | |
| Harry Bixler | | Malko Milling & Lighting Co. | | 1 Apr. 1921 | EOC/138 | 256 | | 833 sq. ft. |
| Madilla Brilhart | | Harry Bixler | | 26 Apr. 1920 | 136 | 244 | | |
| Agnes Murray | | J. Frank Switzer | | 23 Nov. 1917 | 131 | 385 | | |
| Charles W. Murray, trustee | Carroll | Irvin S. Leister | Carroll | 30 Dec. 1910 | ODG/116 | 74 | Deed | lot 3 - the elevator, station, and warehouse property - 1 ac. lot 10 - 28,890 sq. ft. \$5,000.00 |
| Agnes A. Murray | | Mandilla C. Brilhart | | 9 Feb. 1910 | 114 | 344 | | |
| Agnes A. Murray | | Harry Bixler | | 3 Feb. 1910 | 113 | 418 | | |
| Charles A. Murray | | Agnes A. Murray | | 31 Dec. 1904 | 101 | 234 | | |
| William A. Murray | | Agnes A. Murray | | 12 May 1899 | 89 | 275 | | |
| Ephraim W. Turner | | William A. Murray | | 29 Jul. 1889 | 69 | 518 | | |
| Keziah Murray | | Charles M. Murray | | 4 Dec. 1882 | 58 | 114 | | |
| Andrew Schaffer | | Charles M. Murray | | 17 Dec. 1878 | | | | |
| William A. Murray | | Agnes A. Murray | | 12 Jun. 1875 | | | | \$800.00 |
| Mary Sykes | | Keziah Murray | | 26 Jul. 1870 | 38 | 435 | | \$1,000.00 |
| Joseph Ebaugh | | Charles M. Murray | | 12 Apr. 1869 | 37 | 77 | | \$5,500.00 |
| Rebecca Jane Cox | | Joseph Ebaugh | | 6 Mar. 1854 | 16 | 154 | | |



CARR-
1261

HAMPSTEAD RAILROAD STATION
GILL AVENUE - HAMPSTEAD

MEASURED BY KEN SHORT AND KEN HANKINS
DRAWN BY KEN SHORT

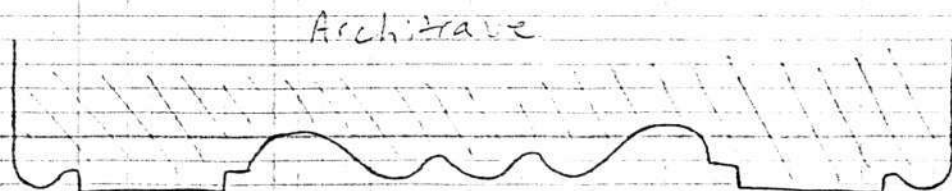
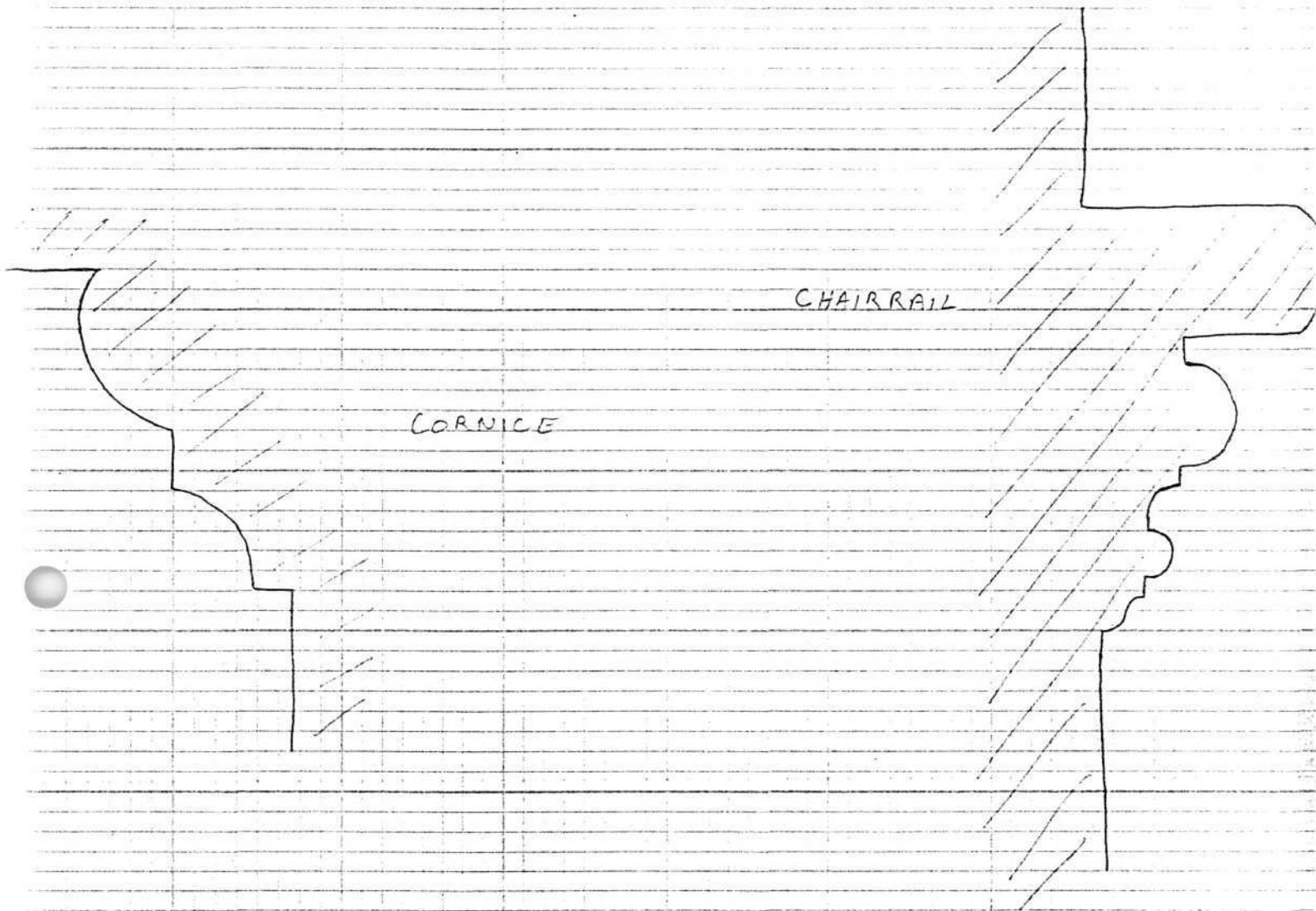
FIRST FLOOR PLAN
JULY 1998

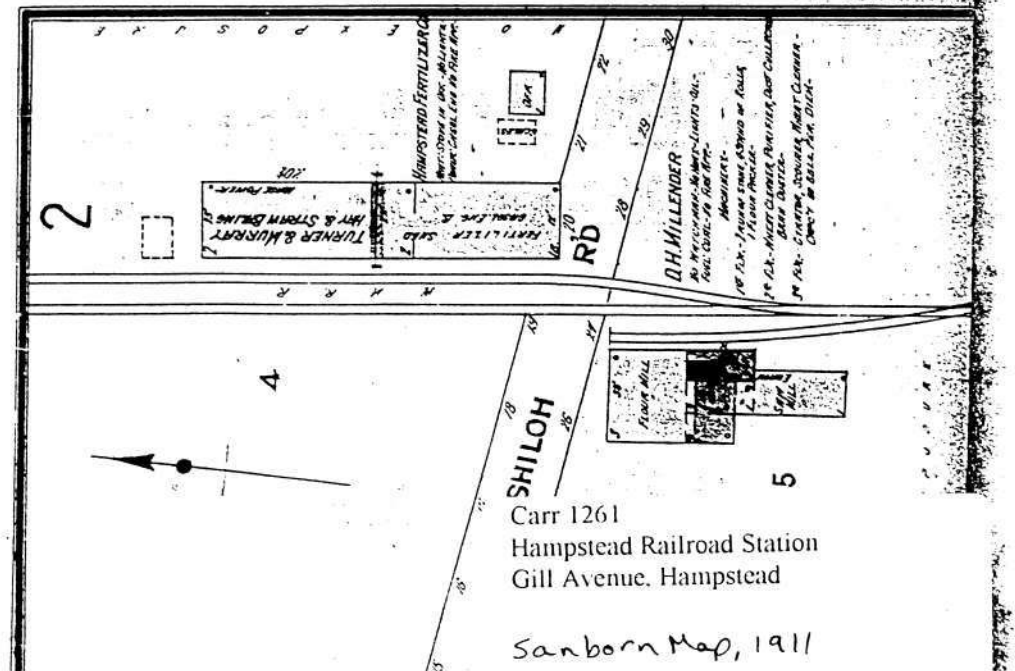
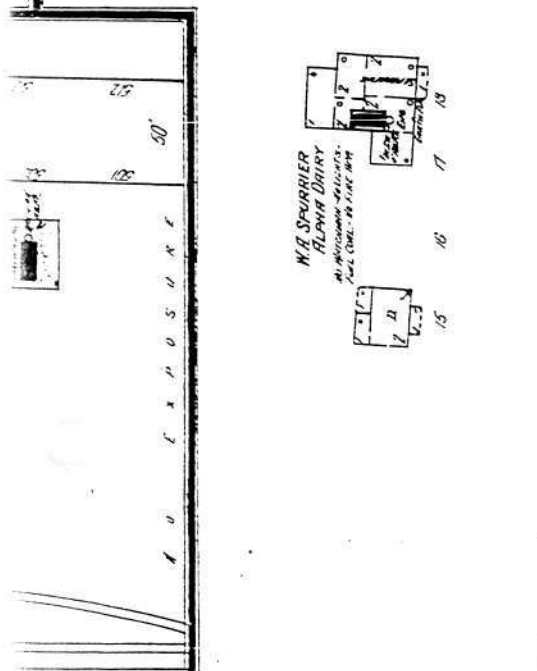
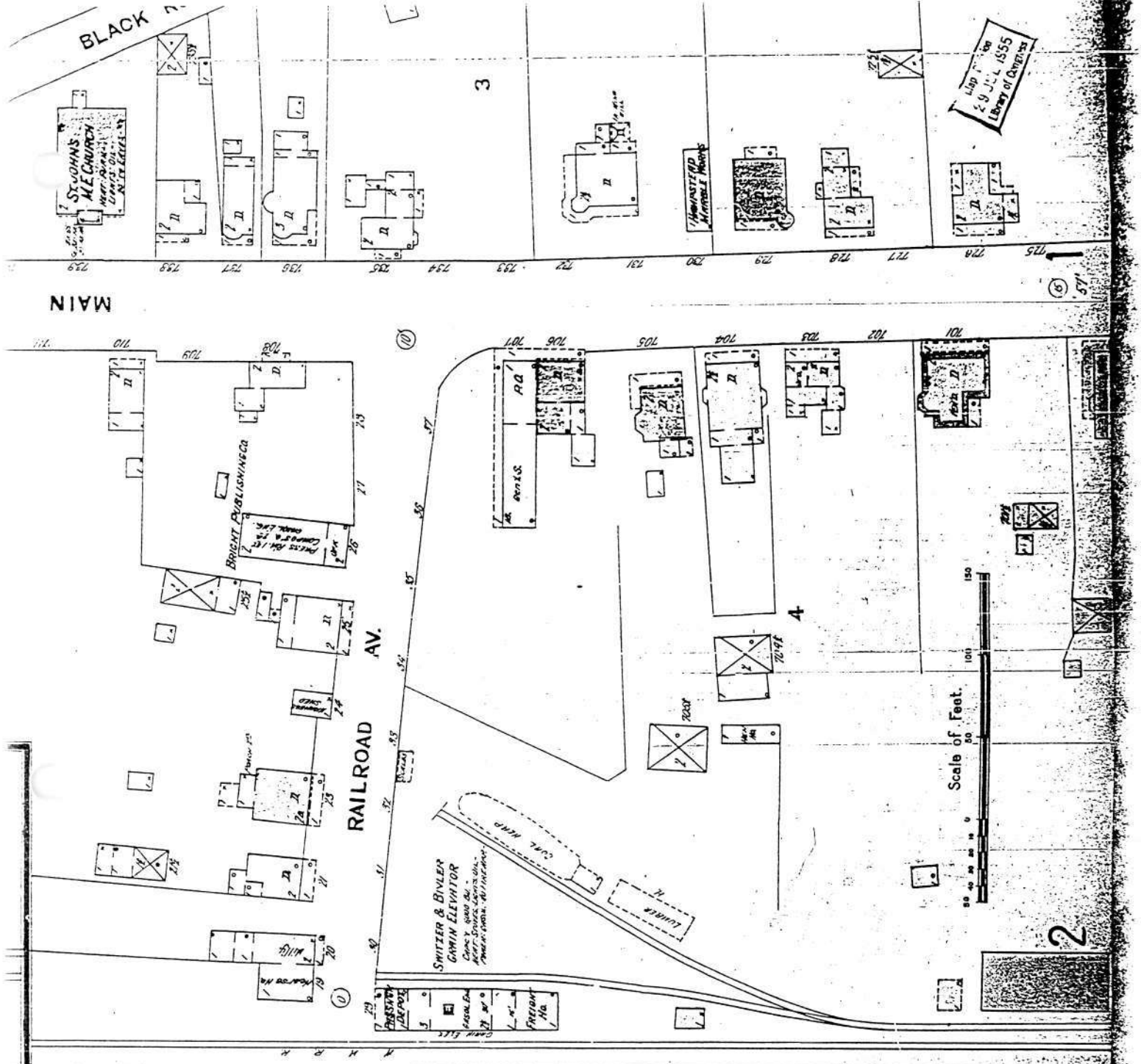


17
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Hampstead Railroad Station Moulding Profiles

KRS
29 Jan '93

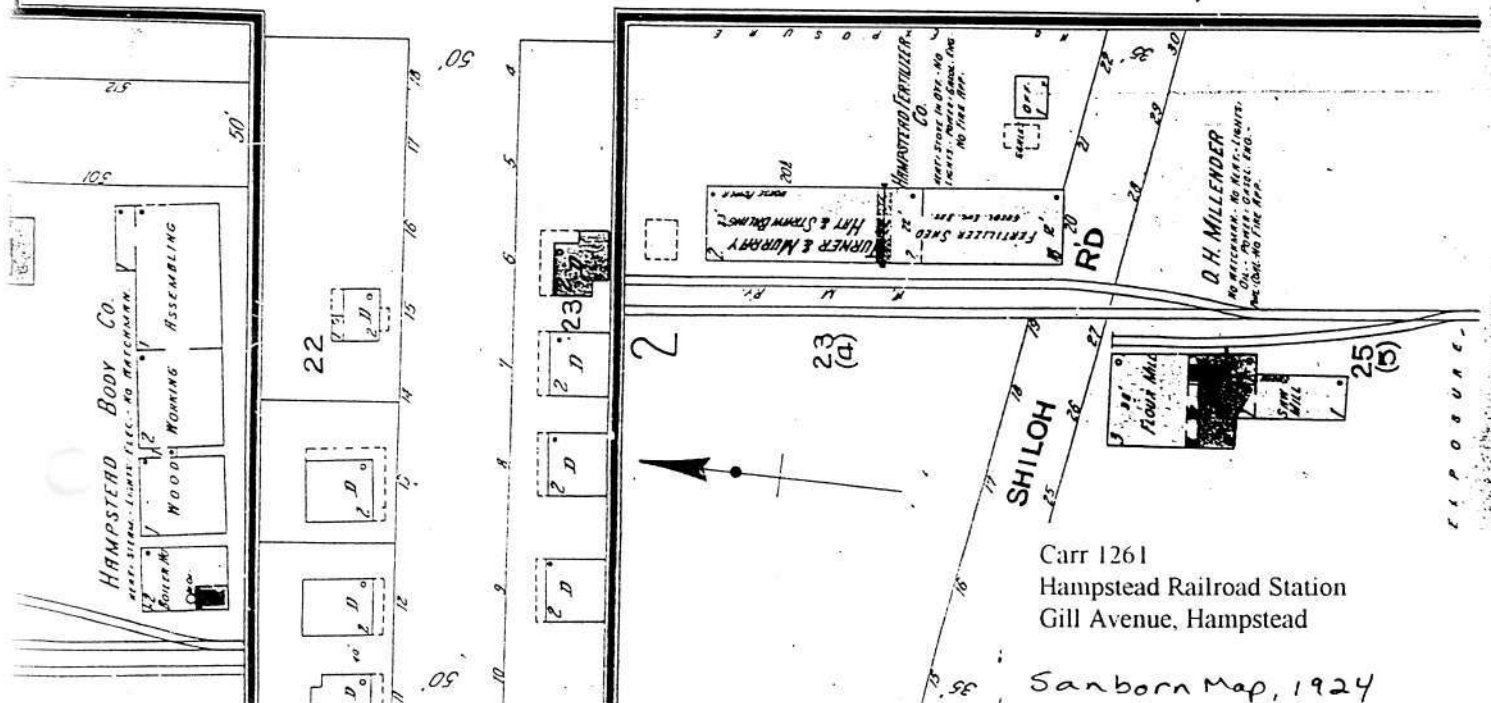
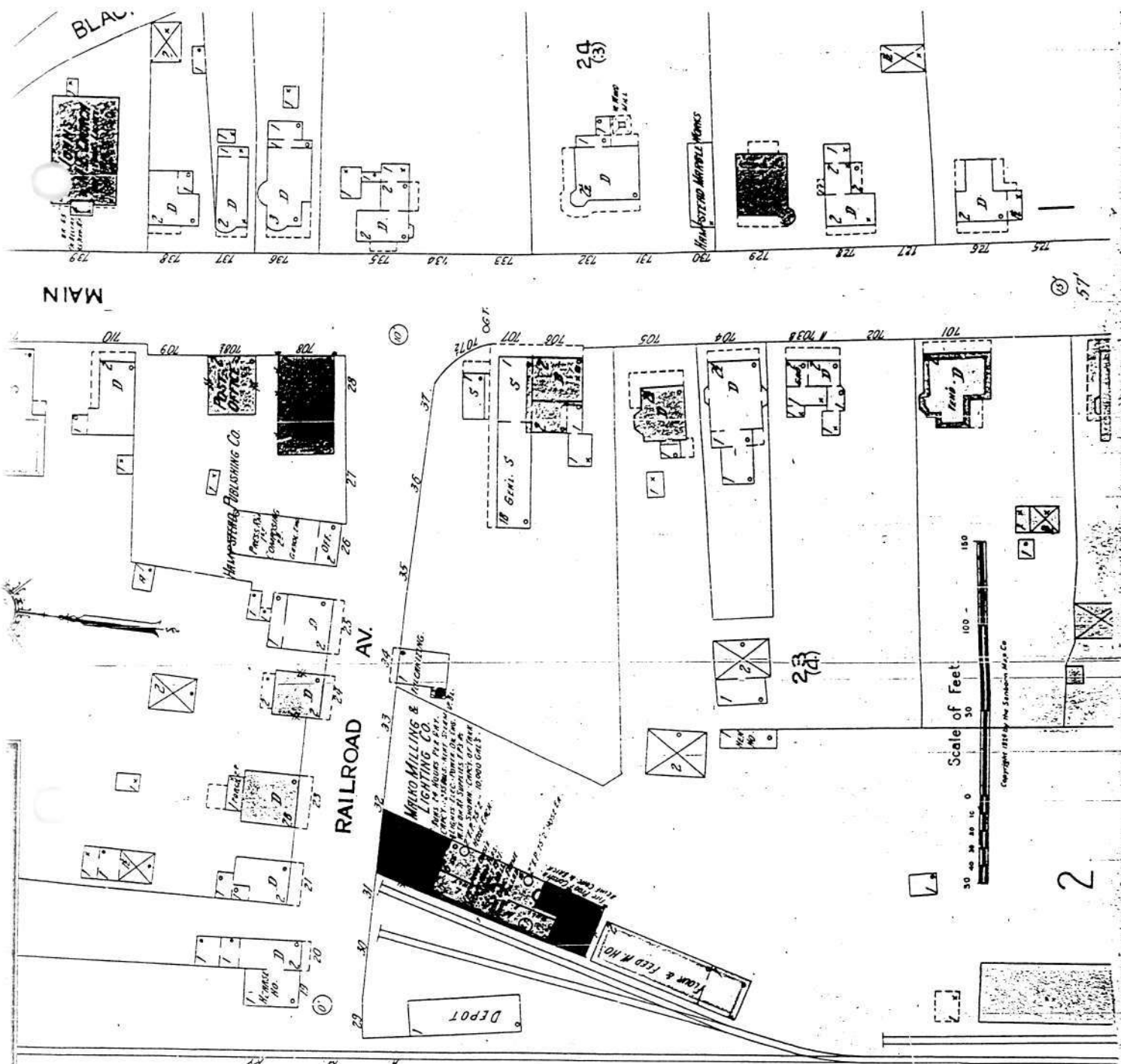




Carr 1261
Hampstead Railroad Station
Gill Avenue, Hampstead

Sanborn Map, 1911

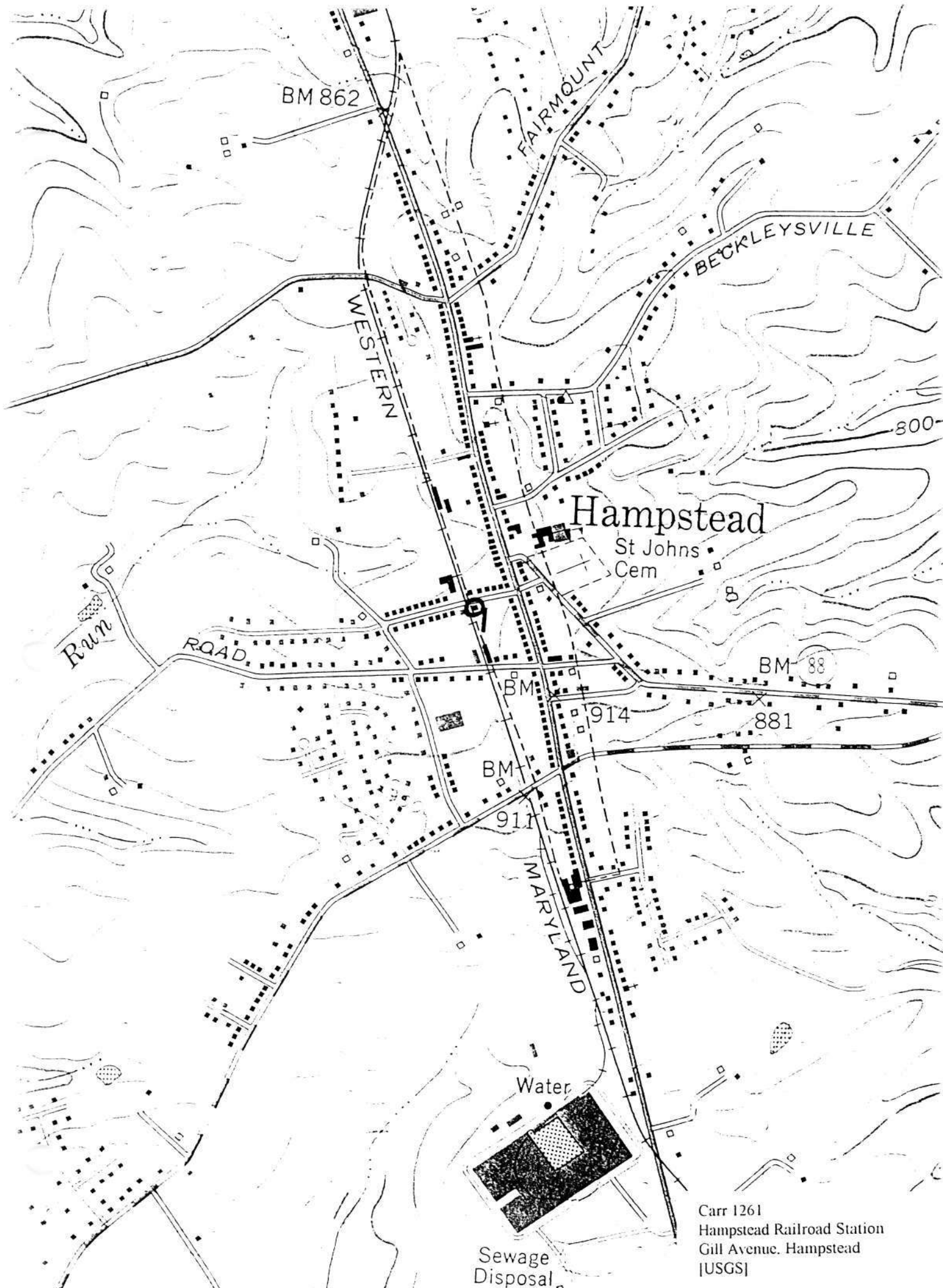
Aug. 1911, 2.



Carr 1261
Hampstead Railroad Station
Gill Avenue, Hampstead

Sanborn Map, 1924

Jul. 1924 2.



Carr 1261
Hampstead Railroad Station
Gill Avenue, Hampstead
[USGS]



CARR-1261

HAMPSTEAD RAILROAD STATION

GILL AVENUE

HAMPSTEAD, CARROLL COUNTY, MARYLAND

PHOTO: KENNETH M. SHORT

MPS4NO.23 X18 21** N N N-9-322

DATE JAN. '98

NEG. LOC: MARYLAND HISTORICAL TRUST

SOUTH & EAST ELEVATIONS

10F4



CARR-1261

HAMPSTEAD RAILROAD STATION

GILL AVENUE

HAMPSTEAD, CARROLL COUNTY, MARYLAND

PHOTO: KENNETH M. SHORT

MPS4NO.22 X218 21** N N N-B-322

DATE: JAN. '98

NEG LOC: MARYLAND HISTORICAL TRUST

NORTH & WEST ELEVATIONS

2 OF 4



CARR-1261

HAMPSTEAD RAILROAD STATION

GILL AVENUE

HAMPSTEAD, CARROLL COUNTY, MARYLAND

PHOTO: KENNETH M. SHORT

MP54NO.21 X21B 21** N N N-9 162

DATE: JAN '98

NEG. LOC. MARYLAND HISTORICAL TRUST
NORTH CENTER ROOM - VW WEST

3 OF 4



CARR-1261

HAMPSTEAD RAILROAD STATION
GILL AVENUE
HAMPSTEAD, CARROLL COUNTY, MARYLAND
PHOTO: KENNETH M. SHORT

MPS4NO.28 X218 21** N N N-8-842

DATE JAN. '98

NEG. LOC. MARYLAND HISTORICAL TRUST
NORTH ROOM - VW NORTH

4 OF 4

CARR - 1261

HAMPSTEAD RAILROAD STATION

1912

Hampstead

private

The Hampstead Railroad Station is representative of the rural "Stick-Style" railroad stations popular in the late 19th century. Its one-story height, hipped roof with wide eaves, and wood ornamentation reflect the Victorian period during which the railroad arrived in Hampstead. According to Chessie System officials, the station was built in 1912, but local residents feel that it dates to the late 19th century. As the center for commercial development in Hampstead, it has served as the symbol for progress within the community.

Magi No.

DOE ___yes ___no

Maryland Historical Trust State Historic Sites Inventory Form

1. Name (indicate preferred name)

historic Hampstead Railroad Station

and/or common

2. Location

street & number Gill Avenue at Railroad Tracks ___ not for publication

city, town Hampstead ___ vicinity of congressional district Sixth

state Maryland county Carroll

3. Classification

| Category | Ownership | Status | Present Use |
|----------------------|---------------------------|-----------------------|--|
| ___ district | ___ public | <u>X</u> occupied | ___ agriculture ___ museum |
| <u>X</u> building(s) | <u>X</u> private | ___ unoccupied | ___ commercial ___ park |
| ___ structure | ___ both | ___ work in progress | ___ educational ___ private residence |
| ___ site | Public Acquisition | Accessible | ___ entertainment ___ religious |
| ___ object | ___ in process | ___ yes: restricted | ___ government ___ scientific |
| | ___ being considered | ___ yes: unrestricted | ___ industrial <u>X</u> transportation |
| | <u>X</u> not applicable | <u>X</u> no | ___ military ___ other: |

4. Owner of Property (give names and mailing addresses of all owners)

name Chessie System Railroads

street & number 100 North Charles Street telephone no.: 237-2000

city, town Baltimore state and zip code MD

5. Location of Legal Description

courthouse, registry of deeds, etc. Carroll County Courthouse Annex liber

street & number 55 North Court Street folio

city, town Westminster state MD

6. Representation in Existing Historical Surveys

title None

date ___ federal ___ state ___ county ___ local

depository for survey records

city, town state

7. Description

Survey No. CARR-1261

Condition

☐ excellent
☐ good
☒ fair

☐ deteriorated
☐ ruins
☐ unexposed

Check one

☒ unaltered
☐ altered

Check one

☒ original site
☐ moved date of move _____

Prepare both a summary paragraph and a general description of the resource and its various elements as it exists today.

The Hampstead Railroad Station is a one story structure, rectangular in shape, five bays long by two bays wide. This building was built on a concrete foundation. The two main entrances are on either side of the off centered gable that's facing the railroad tracks. The gable decorated with "Kings post" trusses. This section of the building extends out from the frame which suggests that it was used as a pick-up drop-off point, especially considering that it almost completely open from the front. Both side gabled roofs, with over hanging eaves and brace supports, were also adorned with "Kings post" trusses and a big light hand before the side facing the street. Both sides of this building have two windows are are decorated with diagonal, flat stickwork. The details on this building, especially the elaborated gable roof, suggest that it is a Victorian "Stick," popular between 1860 and 1890.

8. Significance

Survey No. CARR-1261

| Period | Areas of Significance—Check and justify below | | | |
|--------------------------------------|---|---|---|--|
| <input type="checkbox"/> prehistoric | <input type="checkbox"/> archeology-prehistoric | <input type="checkbox"/> community planning | <input type="checkbox"/> landscape architecture | <input type="checkbox"/> religion |
| <input type="checkbox"/> 1400-1499 | <input type="checkbox"/> archeology-historic | <input type="checkbox"/> conservation | <input type="checkbox"/> law | <input type="checkbox"/> science |
| <input type="checkbox"/> 1500-1599 | <input type="checkbox"/> agriculture | <input type="checkbox"/> economics | <input type="checkbox"/> literature | <input type="checkbox"/> sculpture |
| <input type="checkbox"/> 1600-1699 | <input type="checkbox"/> architecture | <input type="checkbox"/> education | <input type="checkbox"/> military | <input type="checkbox"/> social/ |
| <input type="checkbox"/> 1700-1799 | <input type="checkbox"/> art | <input type="checkbox"/> engineering | <input type="checkbox"/> music | <input type="checkbox"/> humanitarian |
| <input type="checkbox"/> 1800-1899 | <input type="checkbox"/> commerce | <input type="checkbox"/> exploration/settlement | <input type="checkbox"/> philosophy | <input type="checkbox"/> theater |
| <input type="checkbox"/> 1900- | <input type="checkbox"/> communications | <input type="checkbox"/> industry | <input type="checkbox"/> politics/government | <input type="checkbox"/> transportation |
| | | <input type="checkbox"/> invention | | <input type="checkbox"/> other (specify) |

Specific dates**Builder/Architect**check: Applicable Criteria: ☐ A ☐ B ☐ C ☐ D

and/or

Applicable Exception: ☐ A ☐ B ☐ C ☐ D ☐ E ☐ F ☐ GLevel of Significance: ☐ national ☐ state ☐ local

Prepare both a summary paragraph of significance and a general statement of history and support.

According to Chessie System officials, this station was built in 1912 (Carroll County Times, 4/4/86), although local residents believe that it is a 19th century station (which might be true).

The building is of a rural Victorian railroad station design, fairly plain and simple but with "Stick-style" ornamentation in the gables. As the center of late 19th century commercial development in Hampstead, it has served as the symbol of progress within the community.

9. Major Bibliographical References

Survey No. CARR-1261

Land Records of Carroll County, Maryland
"Hampstead Moves to Preserve 'Symbol'" Carroll County Times. April
4, 1986

10. Geographical Data

Acreage of nominated property _____

Quadrangle name Hampstead

Quadrangle scale _____

UTM References do NOT complete UTM references

A

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Zone Easting Northing

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Verbal boundary description and justification

List all states and counties for properties overlapping state or county boundaries

| state | code | county | code |
|-------|------|--------|------|
|-------|------|--------|------|

| state | code | county | code |
|-------|------|--------|------|
|-------|------|--------|------|

11. Form Prepared By

name/title David DanteWestern Maryland College Internshiporganization Carroll County Dept. of Planning & Development date June 1986street & number 225 N. Center Streettelephone 848-4500city or town Westminsterstate MD

The Maryland Historic Sites Inventory was officially created by an Act of the Maryland Legislature to be found in the Annotated Code of Maryland, Article 41, Section 181 KA, 1974 supplement.

The survey and inventory are being prepared for information and record purposes only and do not constitute any infringement of individual property rights.

return to: Maryland Historical Trust
Shaw House
21 State Circle
Annapolis, Maryland 21401
(301) 269-2438

HAMPSTEAD, MD. QUADRANGLE

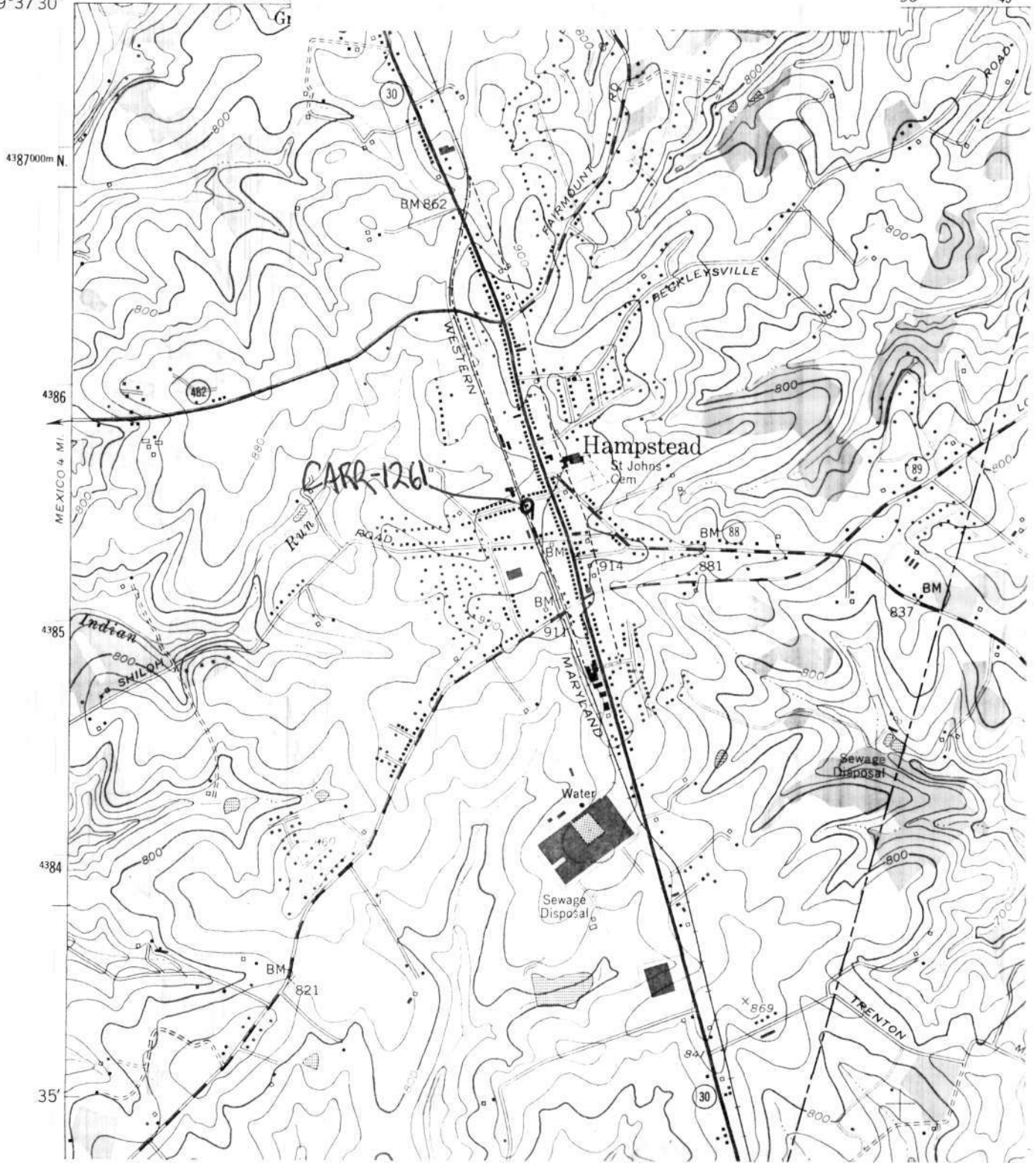
7.5 minute series

5663 III NW
(MANCHESTER)

UNITED
DEPARTMENT OF
GEOLOGICAL

HANOVER
MANCHESTER

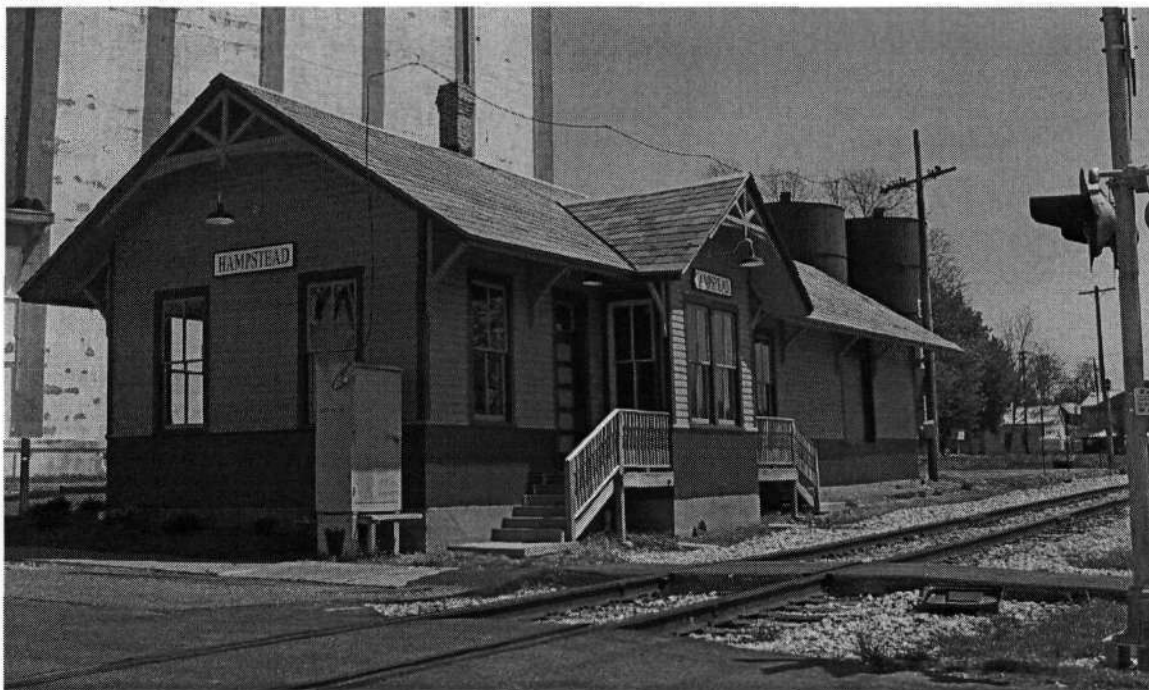
76°52'30"
39°37'30"



CARR-1261
Hampstead Train Station
4035 Gill Avenue, Hampstead
Jennifer K. Cosham, 20 April 2006



North elevation



West elevation